

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

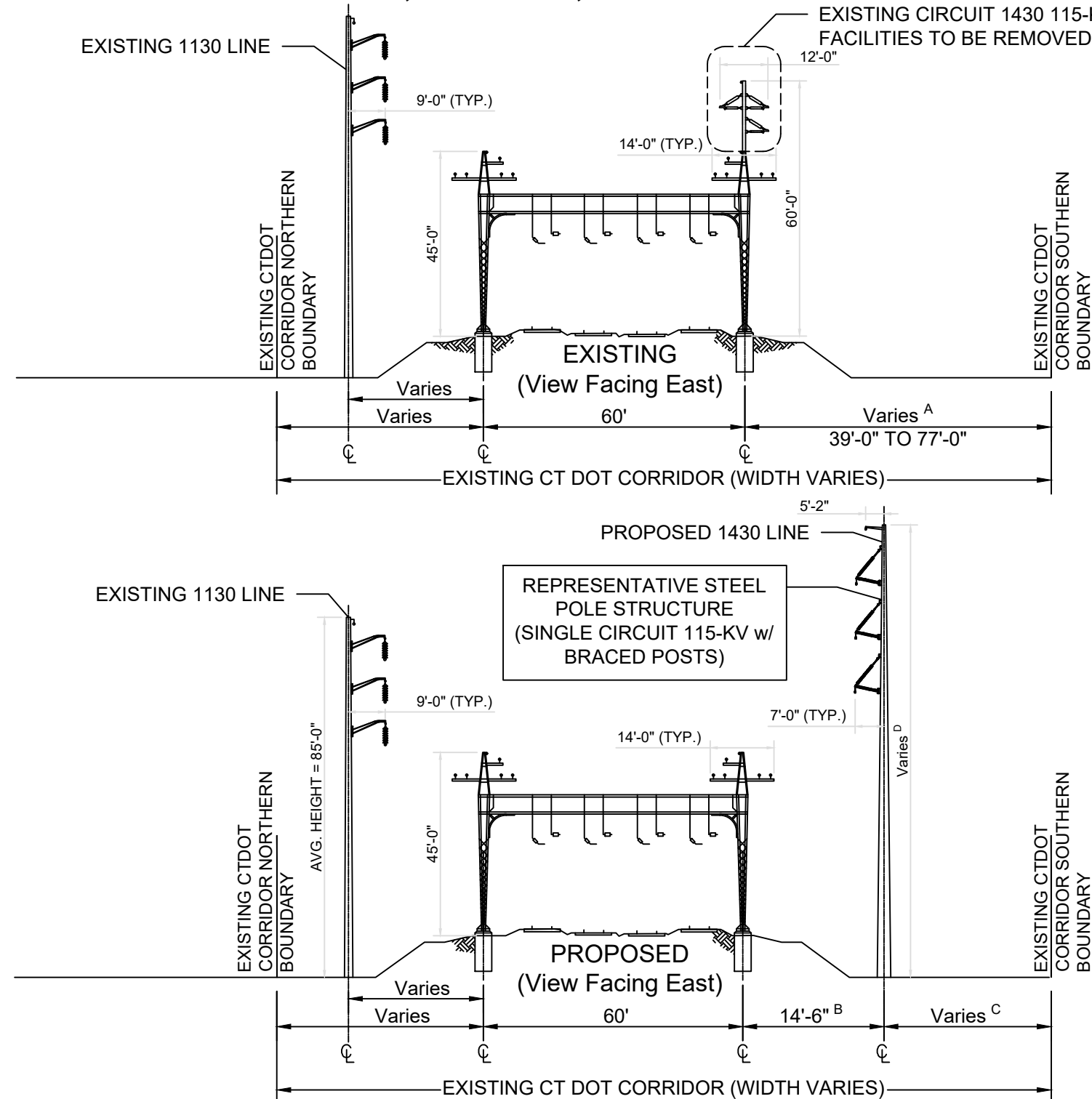
**CROSS SECTION 1**

**EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)**

**STR P648S TO P651S - 0.29 MILES, STR P659S, STR P684S TO STR P686S - 0.25 MILES**

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 14'-6" BUT CAN RANGE FROM 4'-0" TO 21'-6". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

PE Stamp						TYPICAL CROSS SECTION DIAGRAMS			
						SHEET 2 OF 21			
						UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
						DR.	ASW	SCALE: NTS	FILE:
						CK.	MSP	NO.	REV.
						APP.	---	XS-1	0-0C
						DATE:	4/15/2022		

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0-0C	8/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
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0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP

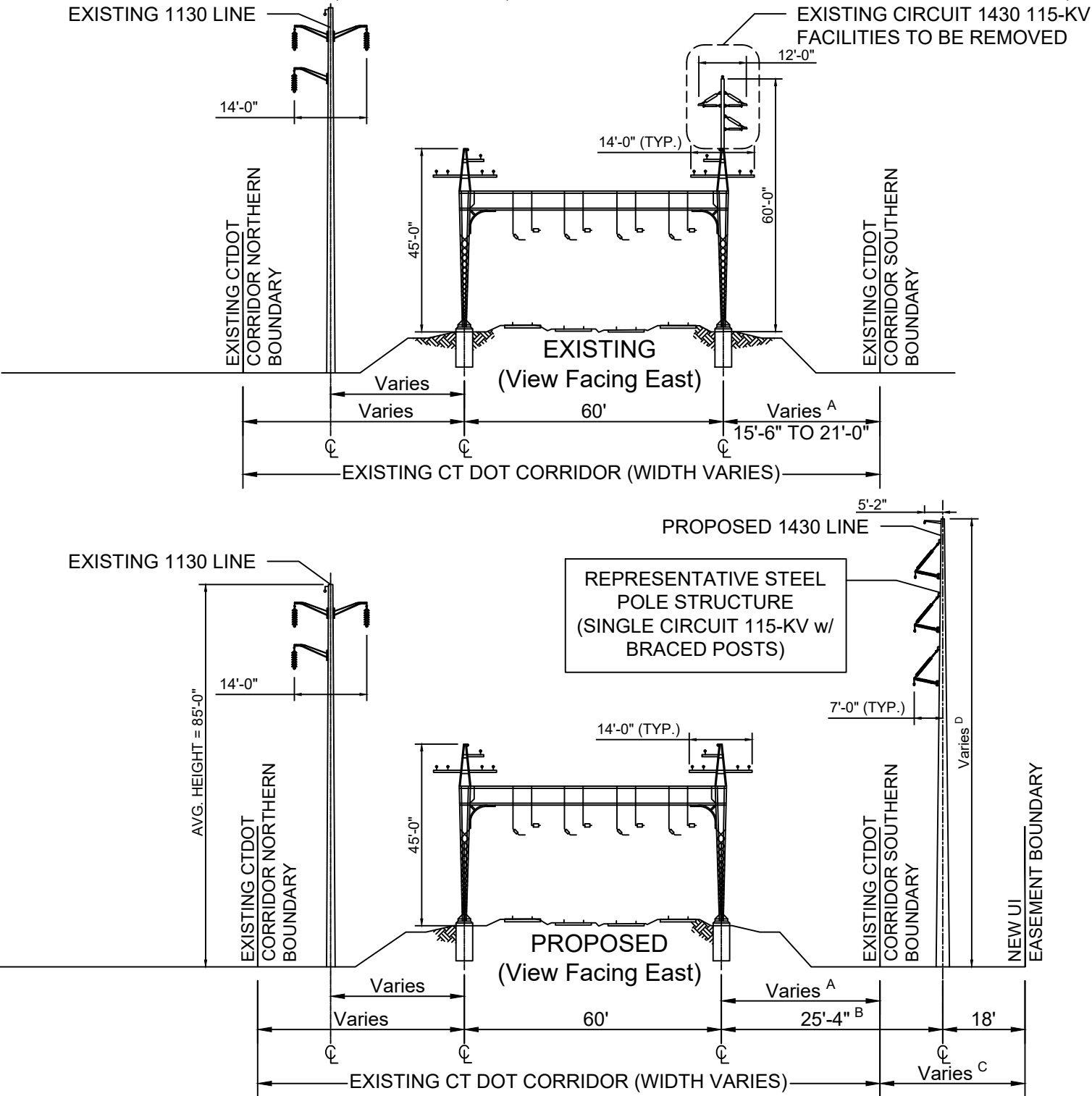
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CROSS SECTION 2  
EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)  
STR P655S TO STR P656S - 0.14 MILES, STR P664S, STR P699S TO STR P703S - 0.30 MILES, STR P708S

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 25'-4" BUT CAN RANGE FROM 17'-0" TO 51'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

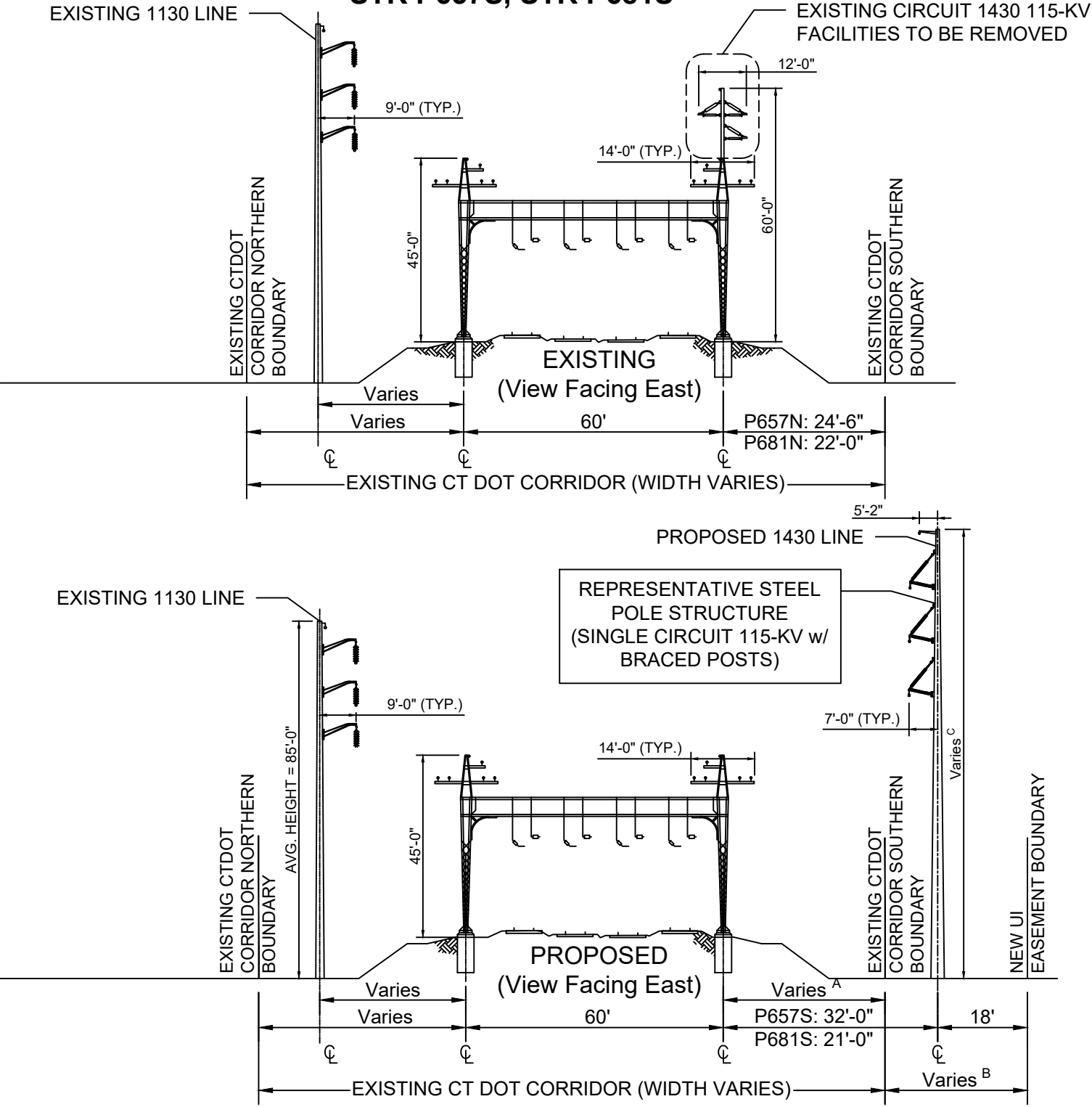
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TYPICAL CROSS SECTION DIAGRAMS									
SHEET 3 OF 21									
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS									
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DATE:	4/15/2022								
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CROSS SECTION 3  
EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)  
STR P657S, STR P681S

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THE WIDTH OF UI'S REQUIRED PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

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												UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS									
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EXISTING 1130 LINE

EXISTING CT DOT CORRIDOR NORTHERN BOUNDARY

9'-0" (TYP.)

45'-0"

60'

EXISTING (View Facing East)

EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY

Varies

Varies<sup>A</sup>

15'-0" to 37'-6"

EXISTING CT DOT CORRIDOR (WIDTH VARIES)

EXISTING CIRCUIT 1430 115-KV FACILITIES TO BE REMOVED

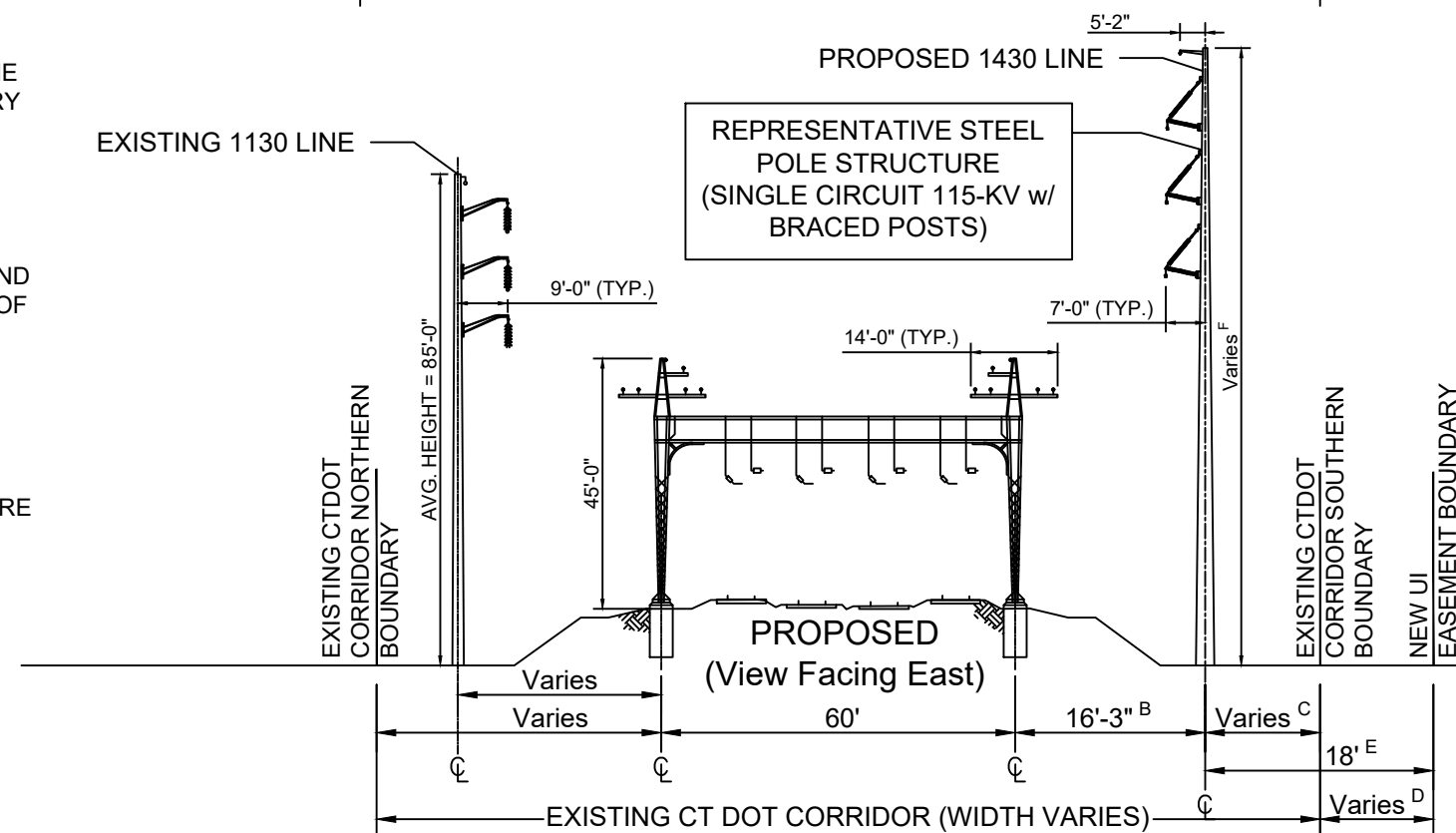
12'-0"

14'-0" (TYP.)

70'-0"

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 32'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NORTH RAILROAD TRACKS.

F THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.



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	0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	REV.	DATE	BY	DESCRIPTION	APP.

TYPICAL CROSS SECTION DIAGRAMS			
SHEET 5 OF 21			
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
DR.	ASW	SCALE: NTS	FILE:
CK.	MSP	NO.	REV.
APP.	---	XS-4	0-0C
DATE:	4/15/2022		

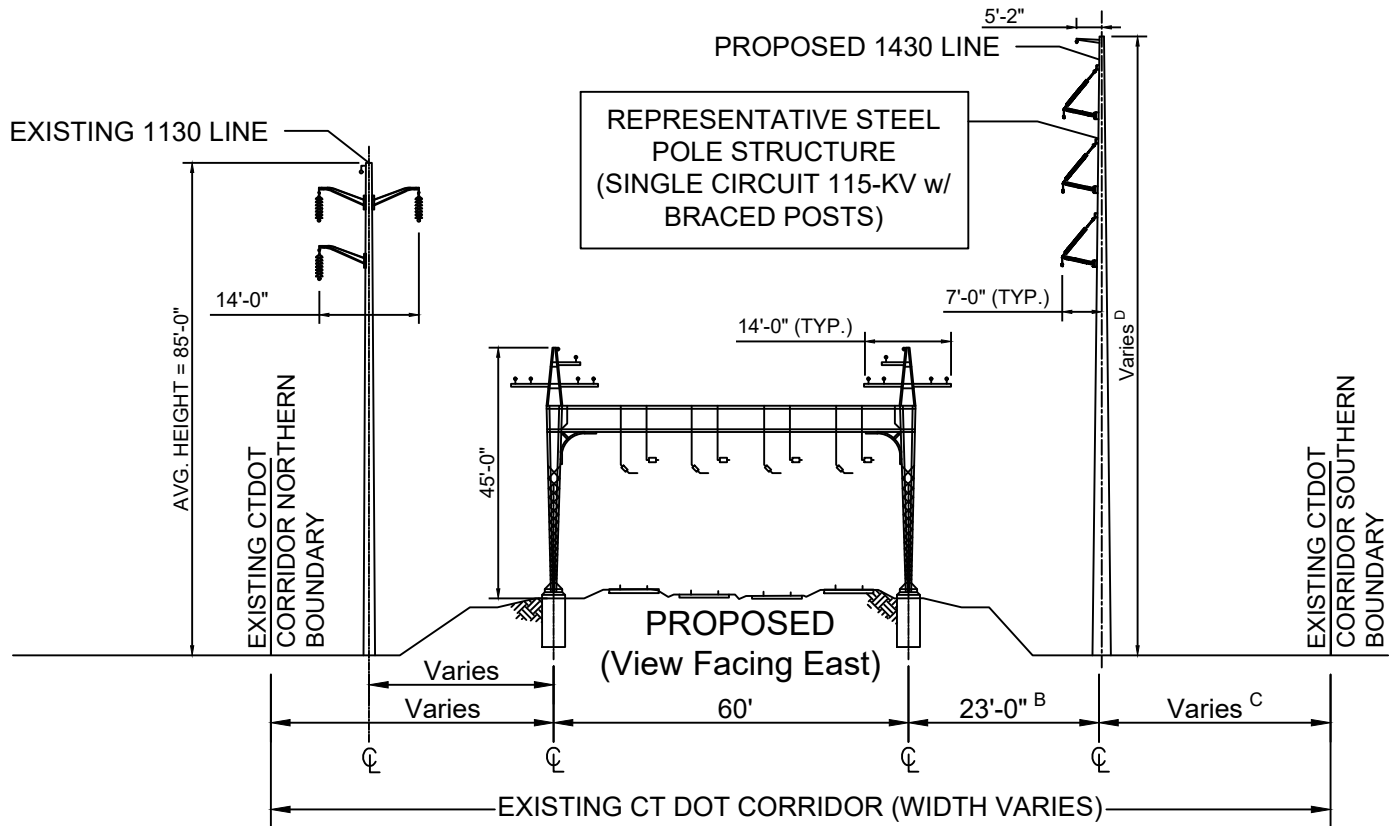
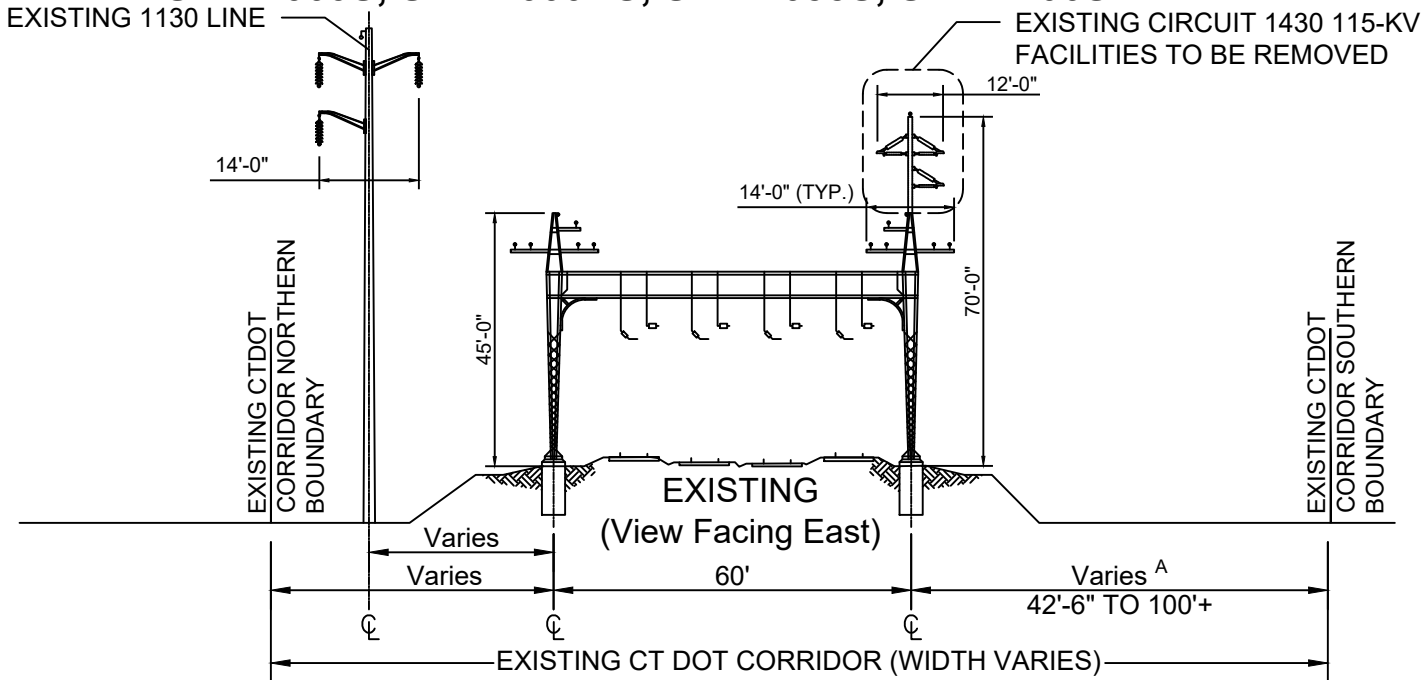
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CROSS SECTION 5  
EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)  
STR P663S, STR P666AS, STR P688S, STR P706S

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
- 2.
3. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
5. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
6. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
7. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
8. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
9. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 23'-0" BUT CAN RANGE FROM 13'-0" TO 50'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

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TYPICAL CROSS SECTION DIAGRAMS					
SHEET 6 OF 21					
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS					
DR.	ASW	SCALE: NTS	FILE:		
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APP.	---	XS-5			0-0C
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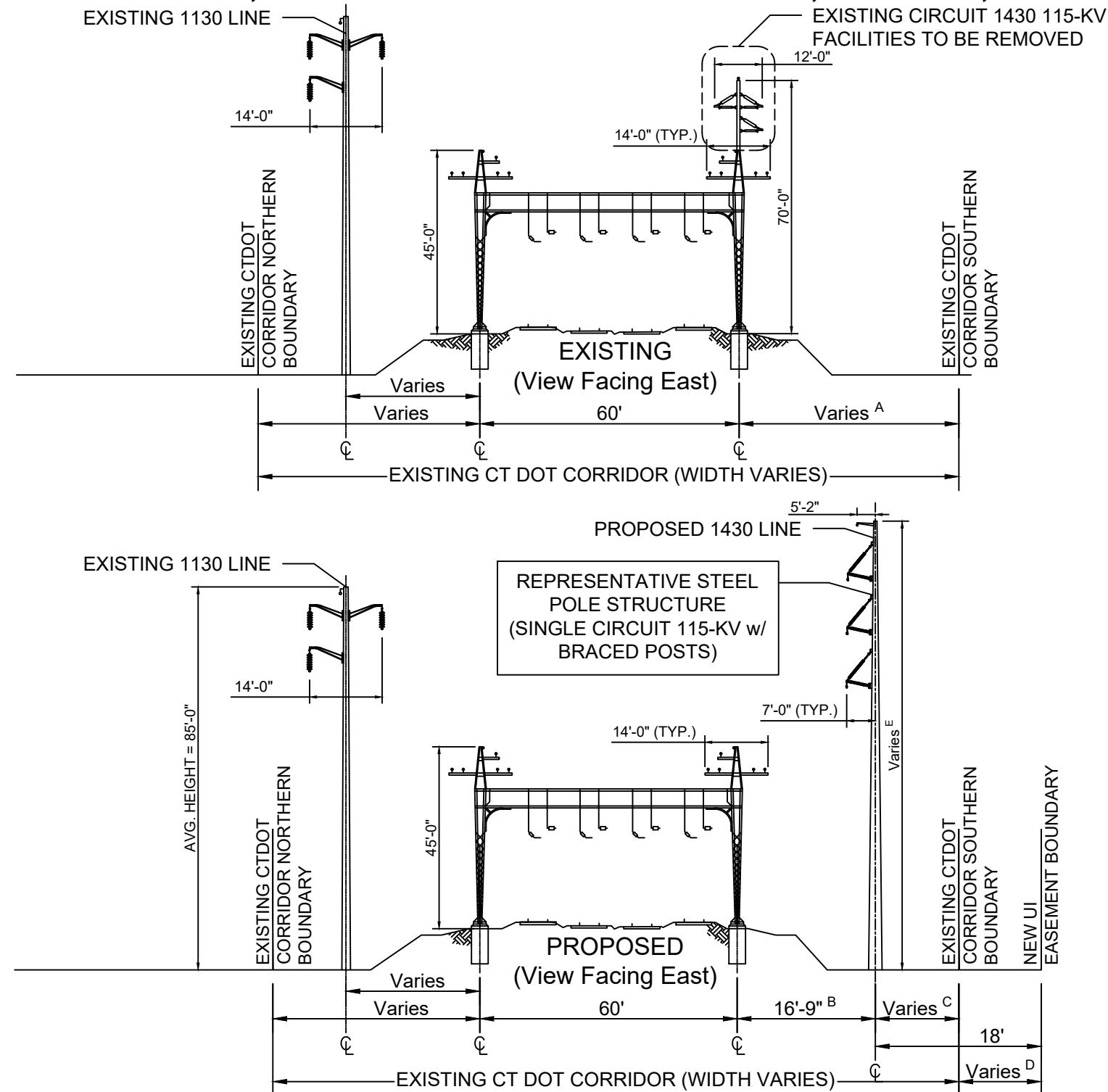
CROSS SECTION 6

EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)

STR P665BS, STR P673S TO STR P678S - 0.45 MILES, STR P689S TO STR P698S - 0.53 MILES, STR P704S, STR P709S TO STR P713S - 0.28 MILES

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 16'-9" BUT CAN RANGE FROM 10'-0" TO 38'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE WIDTH OF UI'S REQUIRED PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>E</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

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TYPICAL CROSS SECTION DIAGRAMS				
SHEET 7 OF 21				
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS				
DR.	ASW	SCALE: NTS	FILE:	
CK.	MSP	NO.		REV.
APP.	---	XS-6		0-0C
DATE:	4/15/2022			

**CROSS SECTION 7**  
**CUT-IN TO ASH CREEK SUBSTATION (FAIRFIELD)**  
**STR P713ES TO STR P713ES-1 & STR P714WS TO STR P714WS-1 - 0.10 MILES**

**NOTES:**

- EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
- TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM CONDUCTOR ATTACHMENT POINTS. IN LOCATIONS WHERE THE EASEMENT UI TRANSMISSION EASEMENT IS GREATER THAN 25'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED AN ADDITIONAL PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES. IN LOCATIONS WHERE THE EXISTING UI TRANSMISSION EASEMENT IS LESS THAN 25'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED ADDITIONAL EASEMENT LOCATIONS.

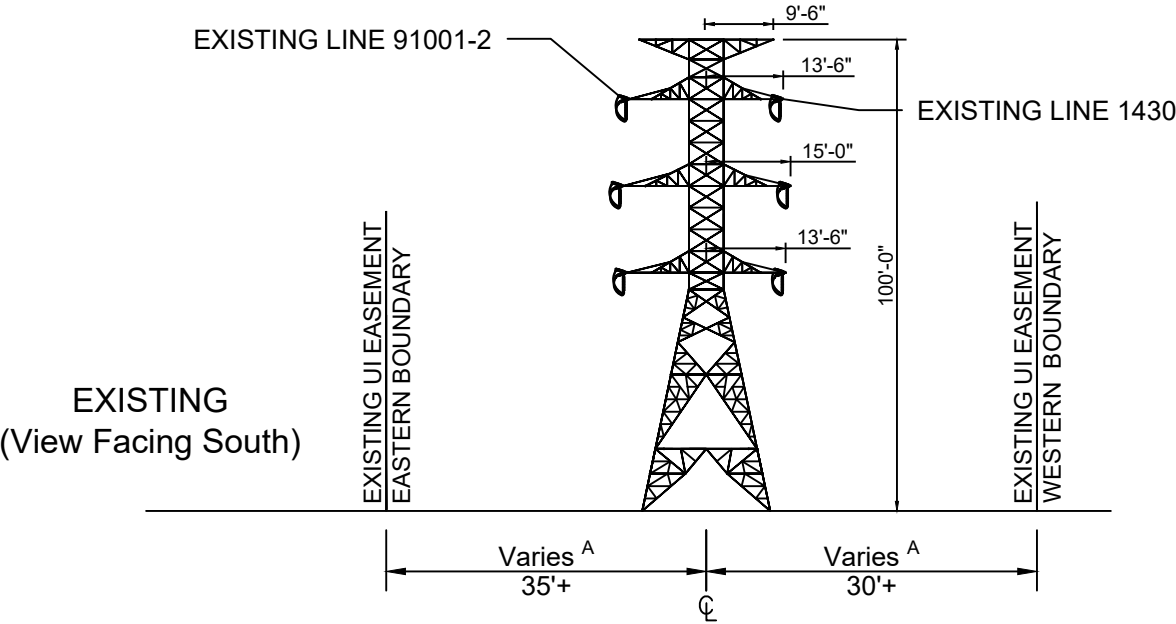
<sup>A</sup> THE WIDTH OF THE EXISTING UI TRANSMISSION EASEMENT IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR THE EXISTING UI TRANSMISSION EASEMENT BOUNDARY INFORMATION.

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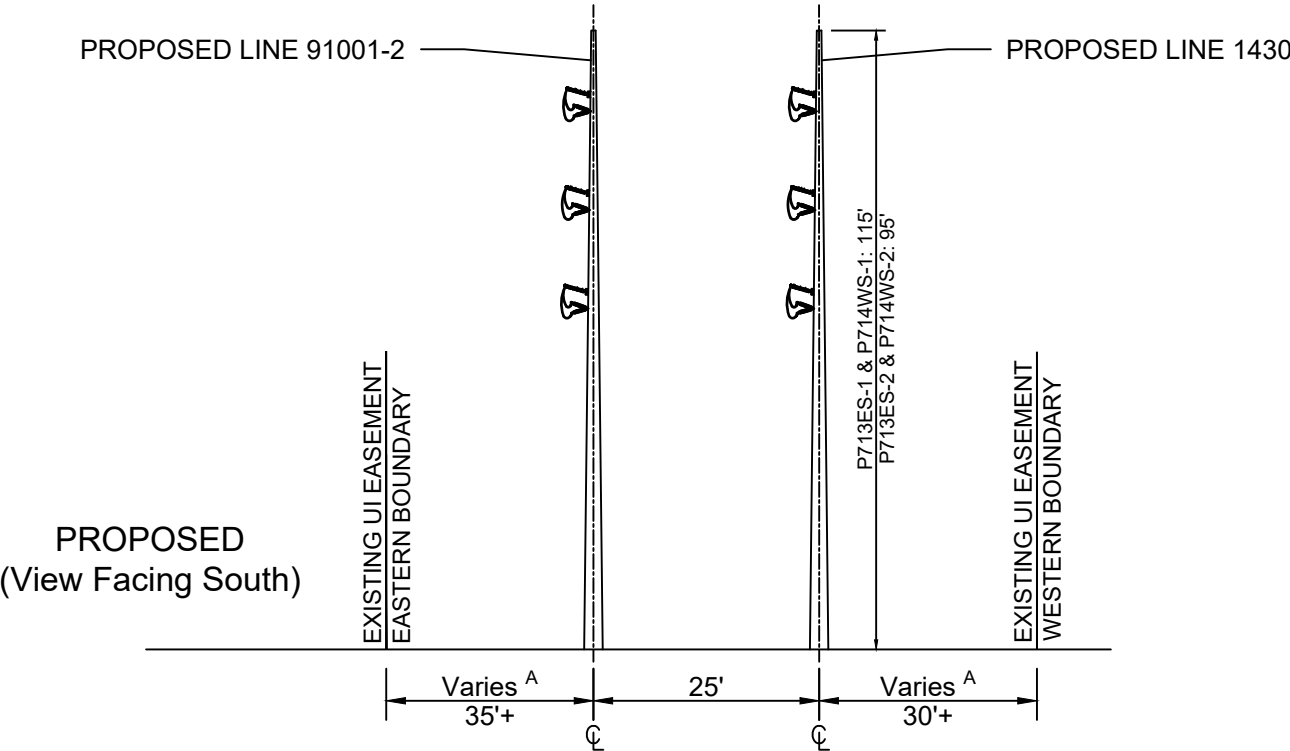
										PE Stamp							TYPICAL CROSS SECTION DIAGRAMS				
REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:						SHEET 8 OF 21								
															UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS						
								DRAWING PREPARED BY:	ACCEPTED BY OE:	0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	DR.	ASW	SCALE: NTS	FILE:	REV. 0-0C		
										0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	CK.	MSP	NO. XS-7				
						0-0A	4/15/2022			WESTWOOD	ISSUE FOR REVIEW	MSP	APP.	---							
						REV.	DATE			BY	DESCRIPTION			APP.	DATE:	4/15/2022					

CROSS SECTION 8  
CUT-IN TO ASH CREEK SUBSTATION (FAIRFIELD AND BRIDGEPORT)  
STR P713ES-1 TO STR P713ES-2 & STR P714WS-1 TO STR P714WS-2 - 0.12 MILES

- NOTES:
- 1. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
  - 2. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM CONDUCTOR ATTACHMENT POINTS. IN LOCATIONS WHERE THE EASEMENT UI TRANSMISSION EASEMENT IS GREATER THAN 25'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED AN ADDITIONAL PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.



<sup>A</sup> THE WIDTH OF THE EXISTING UI TRANSMISSION EASEMENT IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR THE EXISTING UI TRANSMISSION EASEMENT BOUNDARY INFORMATION.



PE Stamp

TYPICAL CROSS SECTION DIAGRAMS

SHEET 9 OF 21

UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS

REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:

DRAWING PREPARED BY: ACCEPTED BY OE:

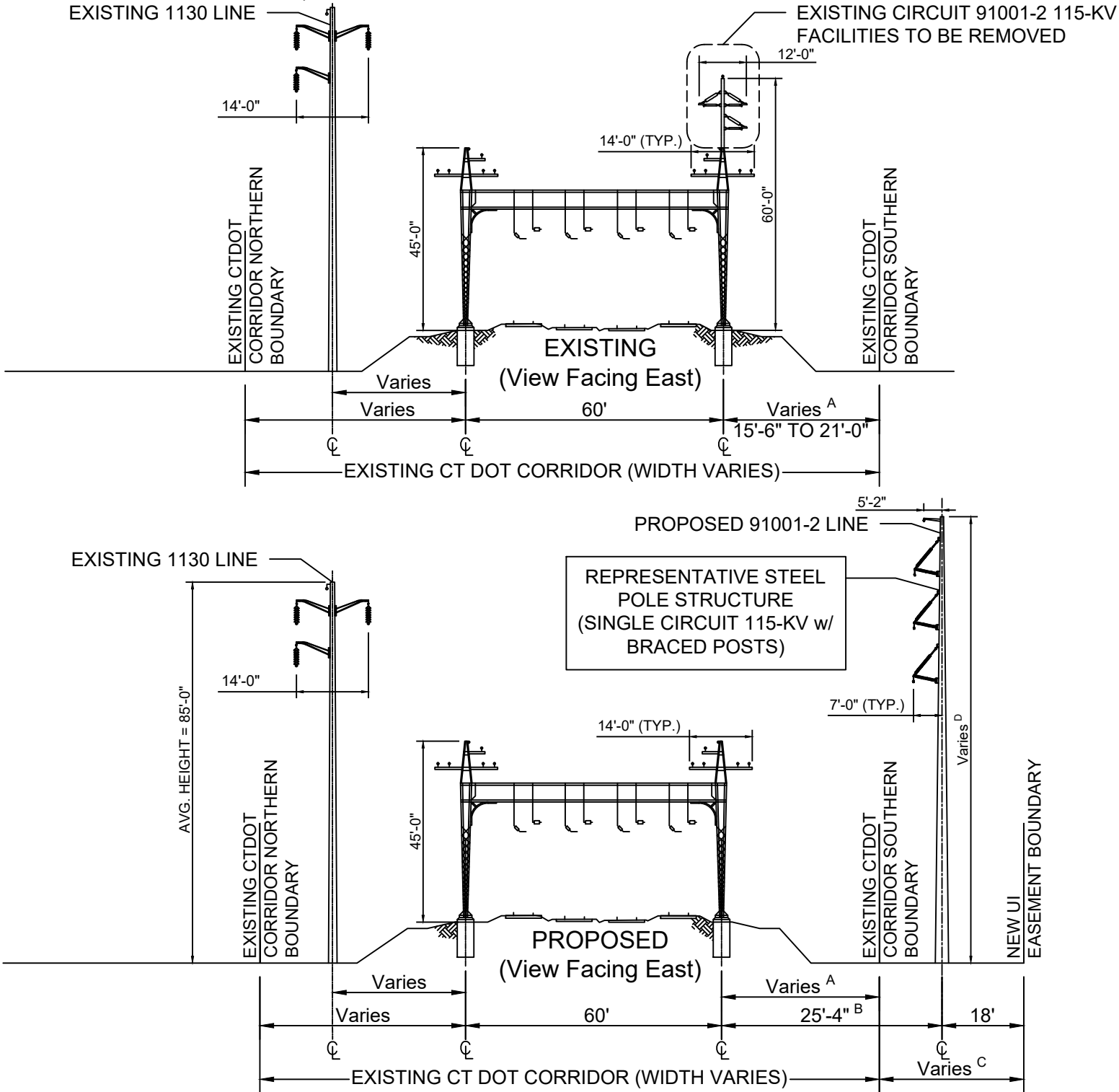
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0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
REV.	DATE	BY	DESCRIPTION	APP.

DR.	ASW	SCALE: NTS	FILE:	REV.
CK.	MSP	NO.		0-0C
APP.	---	XS-8		
DATE:	4/15/2022			



CROSS SECTION 9  
ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)  
STR P716S, STR P721ES TO STR P724S - 0.22 MILES

- NOTES:
1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
  2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
  3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
  4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
  5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
  6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
  7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
  8. EXISTING STEEL POLE ON NORTH SIDE OF METRO NORTH RAILROAD TRACKS WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 25'-4" BUT CAN RANGE FROM 17'-0" TO 51'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

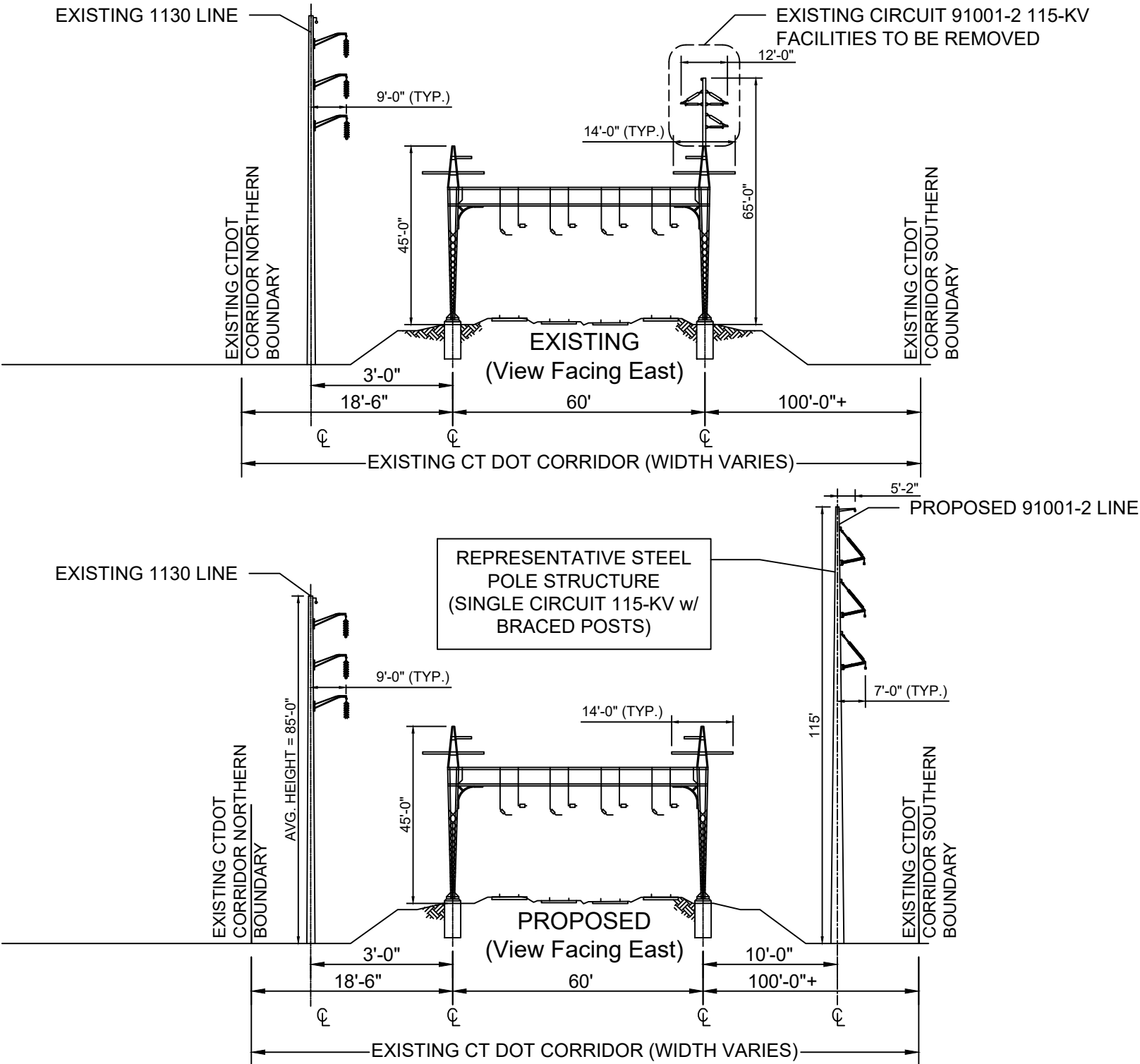
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																SHEET 10 OF 21										
																UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS										
															DR.		ASW		SCALE: NTS		FILE:					
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															DATE:		4/15/2022									
															REV.		DATE		BY		DESCRIPTION				APP.	

CROSS SECTION 10  
ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)  
STR P719S - FAIRFIELD METRO TRAIN STATION

NOTES:

1. THE EXISTING CATENARY STRUCTURES DO NOT SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES IN THE VICINITY OF THE FAIRFIELD METRO TRAIN STATION.
2. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
3. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
4. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
5. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
6. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



PE Stamp

TYPICAL CROSS SECTION DIAGRAMS

SHEET 11 OF 21

UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS

REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:

DRAWING PREPARED BY: ACCEPTED BY OE:

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0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW		MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW		MSP
REV.	DATE	BY	DESCRIPTION	APP.	

DR.	ASW	SCALE: NTS	FILE:		
CK.	MSP	NO.			REV.
APP.	---				
DATE:	4/15/2022	XS-10			0-0C

EXISTING 1130 LINE

EXISTING CT DOT CORRIDOR NORTHERN BOUNDARY

9'-0" (TYP.)

45'-0"

14'-0" (TYP.)

12'-0"

70'-0"

EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY

EXISTING (View Facing East)

Varies

60'

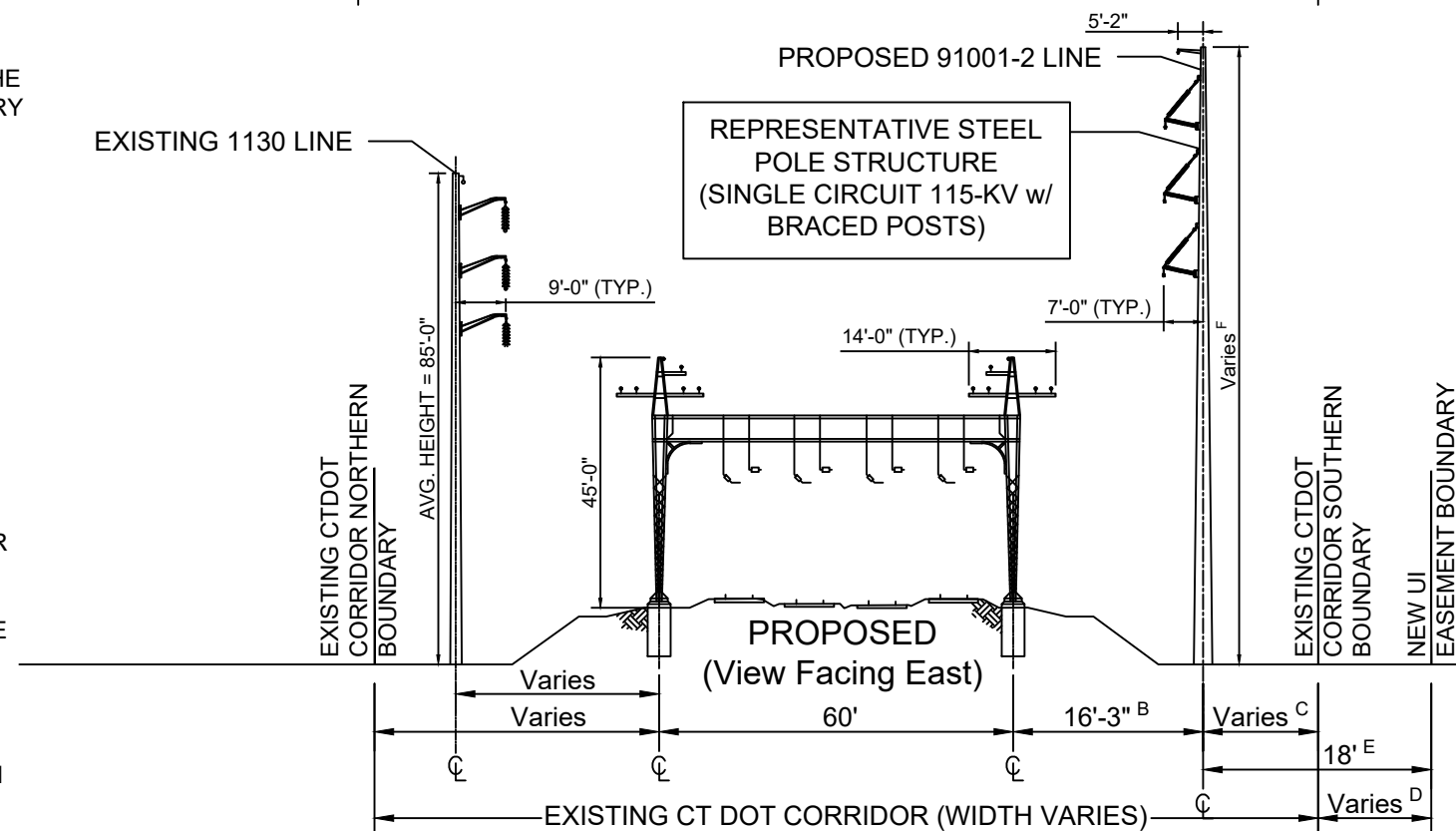
Varies<sup>A</sup>

15'-0" to 37'-6"

EXISTING CT DOT CORRIDOR (WIDTH VARIES)

EXISTING CIRCUIT 91001-2 115-KV FACILITIES TO BE REMOVED

F THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.



REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:
						DRAWING PREPARED BY:	ACCEPTED BY OE:

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	0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	REV.	DATE	BY	DESCRIPTION	APP.

TYPICAL CROSS SECTION DIAGRAMS				
SHEET 12 OF 21				
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS				
DR.	ASW	SCALE: NTS	FILE:	
CK.	MSP	NO.		REV.
APP.	---	XS-11		0-0C
DATE:	4/15/2022			

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EXISTING 1130 LINE

EXISTING CT DOT CORRIDOR NORTHERN BOUNDARY

14'-0"

45'-0"

EXISTING (View Facing East)

60'

70'-0"

12'-0"

EXISTING 91001-2 115-KV FACILITIES TO BE REMOVED

EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY

Varies

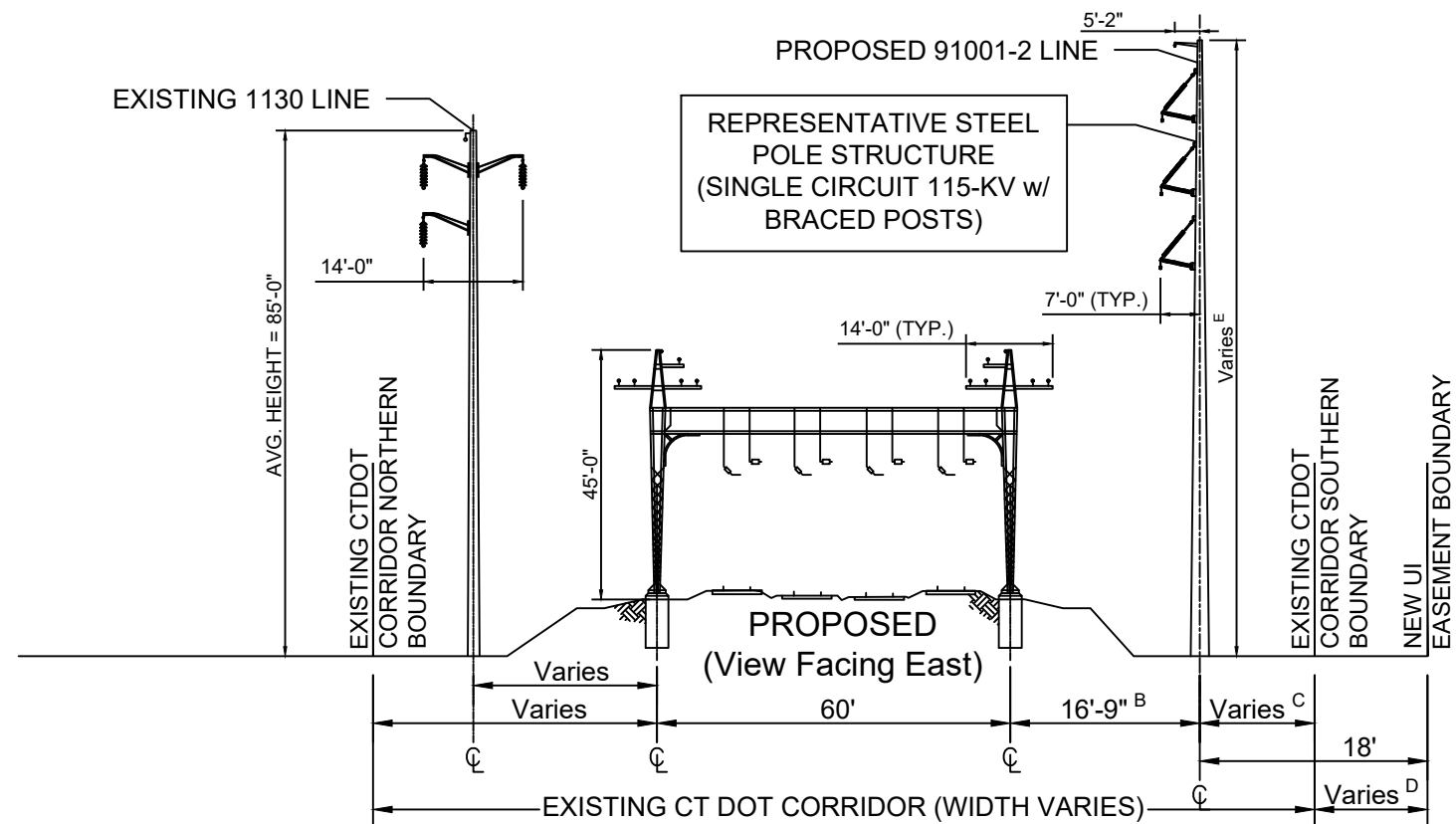
Varies

Varies<sup>A</sup>

EXISTING CT DOT CORRIDOR (WIDTH VARIES)

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.

THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.



REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:
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	0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
	REV.	DATE	BY	DESCRIPTION	APP.

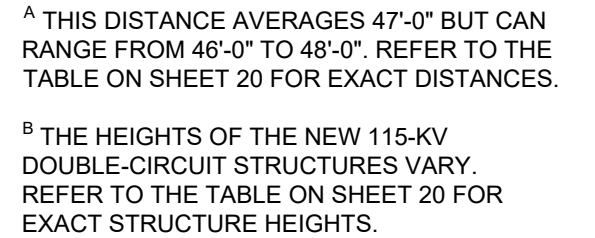
TYPICAL CROSS SECTION DIAGRAMS			
SHEET 13 OF 21			
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
DR.	ASW	SCALE: NTS	FILE:
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DATE:	4/15/2022		

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**CADD Drawing, DO NOT REVISE MANUALLY.**

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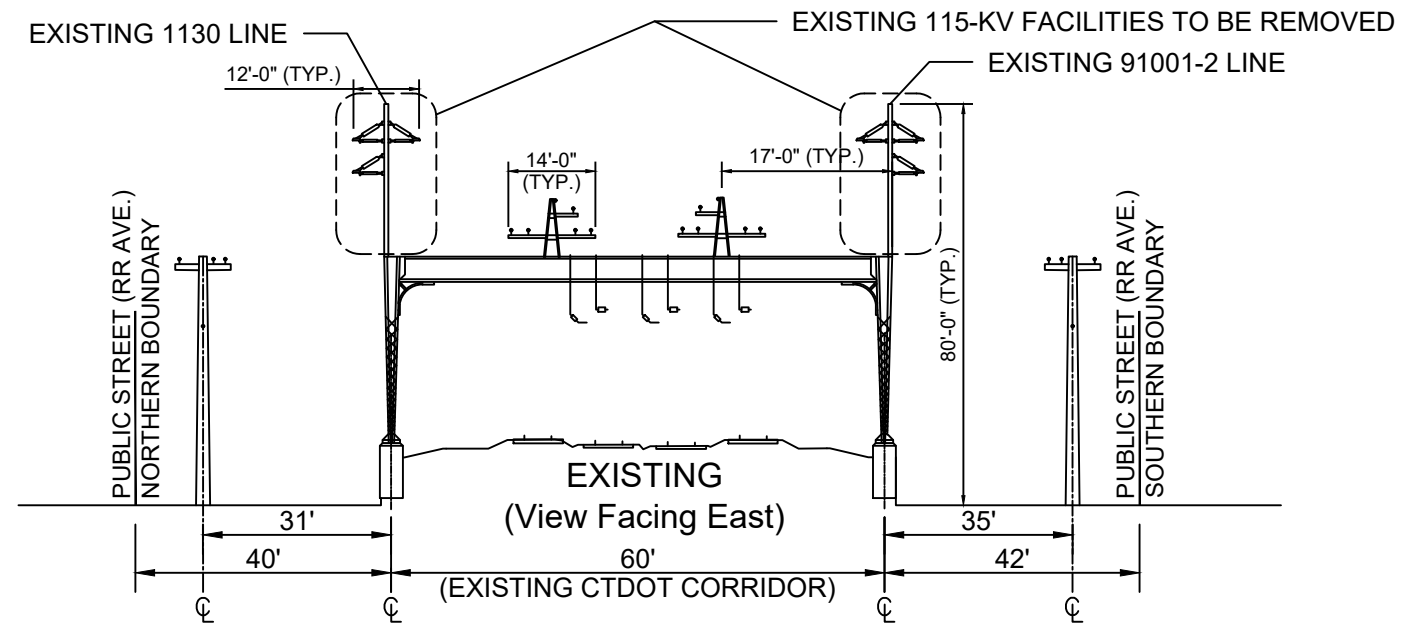


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														UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS					
														DR.	ASW	SCALE: NTS	FILE:		
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CROSS SECTION 14  
ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)  
STR P745S TO STR P752S - 0.40 MILES, STR P762S TO STR P765AS - 0.15 MILES

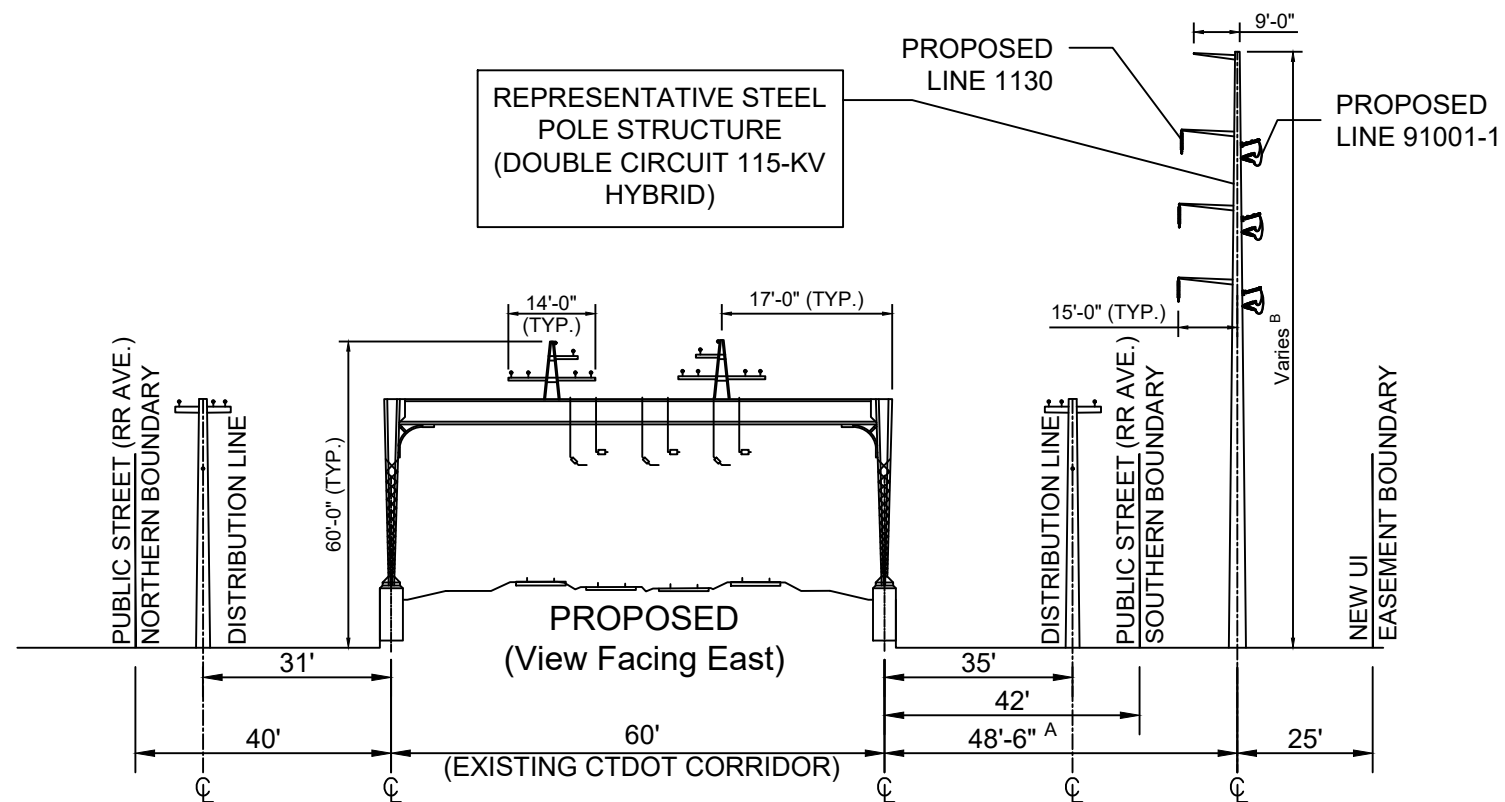
NOTES:

1. DEPICTED STRUCTURES ARE STEEL HYBRID (DEADEND/SUSPENSION) STRUCTURES. TANGENT AND DEADEND STRUCTURES WILL DIFFER.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, INSET 17' FROM BOTH THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
3. THE CT DOT CORRIDOR CONTAINS FOUR (4) ELEVATED RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD, AND IS BOUND ON BOTH NORTH AND SOUTH SIDES BY A PUBLIC STREET (RAILROAD AVENUE.)
4. TO MAINTAIN A.D.A. COMPLIANCE IN CITY SIDEWALKS, IT WAS DEEMED NECESSARY TO PLACE THE PROPOSED MONOPOLES ON PRIVATE PROPERTY, SOUTH OF THE SIDEWALK AND EXISTING UI DISTRIBUTION LINE.
5. UI DISTRIBUTION LINES ON BOTH THE NORTH SIDE AND THE SOUTH SIDE EXIST WITHIN BRIDGEPORT'S RAILROAD AVENUE RIGHT OF WAY. THESE DISTRIBUTION LINES WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.
5. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM EACH STRUCTURE CENTER. IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.



<sup>A</sup> THIS DISTANCE AVERAGES 48'-6" BUT CAN RANGE FROM 36'-0" TO 83'-0". REFER TO THE TABLE ON SHEET 20 FOR EXACT DISTANCES.

<sup>B</sup> THE HEIGHTS OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 20 FOR EXACT STRUCTURE HEIGHTS.



CADD Drawing, DO NOT REVISE MANUALLY.

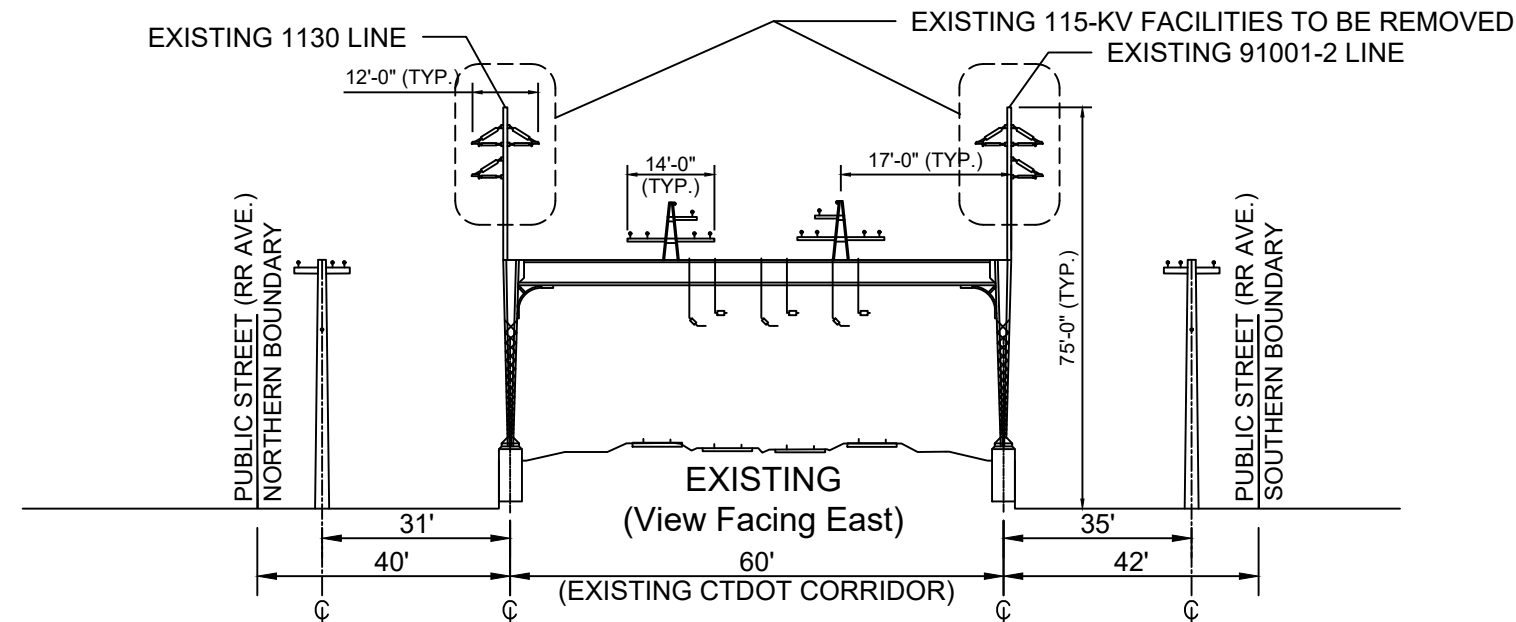
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DESCRIPTION							DATE		BY		CK		SHEET 15 OF 21			
DATE							APP		DRAWING PREPARED BY:		ACCEPTED BY OE:		UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
APP							DR.		SCALE: NTS		FILE:		NO.			
OWNER ENGINEER:							CK.		NO.		REV.		0-0C			
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APPROVAL STAMP:							0-0B		06/24/2022		WESTWOOD		ISSUE FOR REVIEW			
APPROVAL STAMP:							0-0A		4/15/2022		WESTWOOD		ISSUE FOR REVIEW			
APPROVAL STAMP:							REV.		DATE		BY		DESCRIPTION			
APPROVAL STAMP:							0-0C		8/19/2022		WESTWOOD		ISSUE FOR REVIEW			

CROSS SECTION 15  
ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)  
STR P756N TO STR P759N & STR P756S TO STR P760S - 0.19 MILES

NOTES:

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, INSET 17' FROM BOTH THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
3. THE CT DOT CORRIDOR CONTAINS FOUR (4) ELEVATED RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD, AND IS BOUNDED ON BOTH SIDES BY A PUBLIC STREET (RAILROAD AVENUE).
4. TO MAINTAIN A.D.A. COMPLIANCE IN CITY SIDEWALKS, IT WAS DEEMED NECESSARY TO PLACE HE PROPOSED MONOPOLES ON PRIVATE PROPERTY, NORTH OR SOUTH OF THE SIDEWALK AND EXISTING UI DISTRIBUTION LINE. DUE TO THE AMOUNT OF RESIDENTIAL PROPERTIES ADJACENT TO THE NORTH SIDE OF RAILROAD AVENUE IN THIS AREA, IT WAS DEEMED PREFERENTIAL TO LIMIT IMPACTS TO THESE PROPERTIES AND ROUTE THE LINE FURTHER AWAY FROM RAILROAD AVENUE AND CLOSER TO I-95.
5. UI DISTRIBUTION LINES EXIST WITHIN PUBLIC SPACE, BOTH NORTH AND SOUTH OF RAILROAD AVENUE. THESE DISTRIBUTION LINES WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.
6. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM CONDUCTOR ATTACHMENT POINTS. IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.

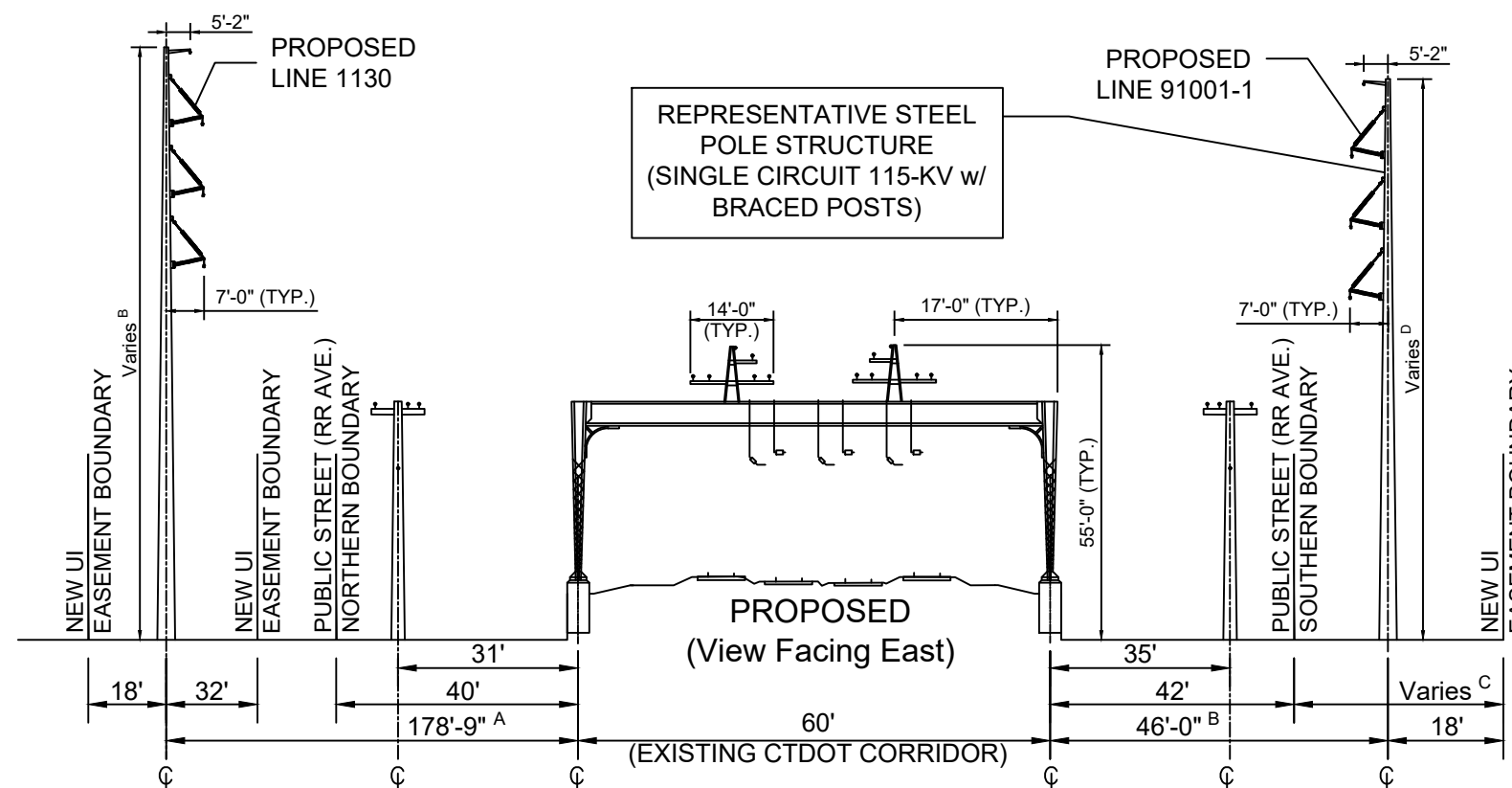


<sup>A</sup> THIS DISTANCE AVERAGES 178'-9" BUT CAN RANGE FROM 57'-0" TO 317'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>B</sup> THIS DISTANCE AVERAGES 46'-0" BUT CAN RANGE FROM 45'-0" TO 48'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 21 FOR EXACT STRUCTURE HEIGHTS.



PE Stamp

TYPICAL CROSS SECTION DIAGRAMS

SHEET 16 OF 21

UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS

REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:

DRAWING PREPARED BY: ACCEPTED BY OE:

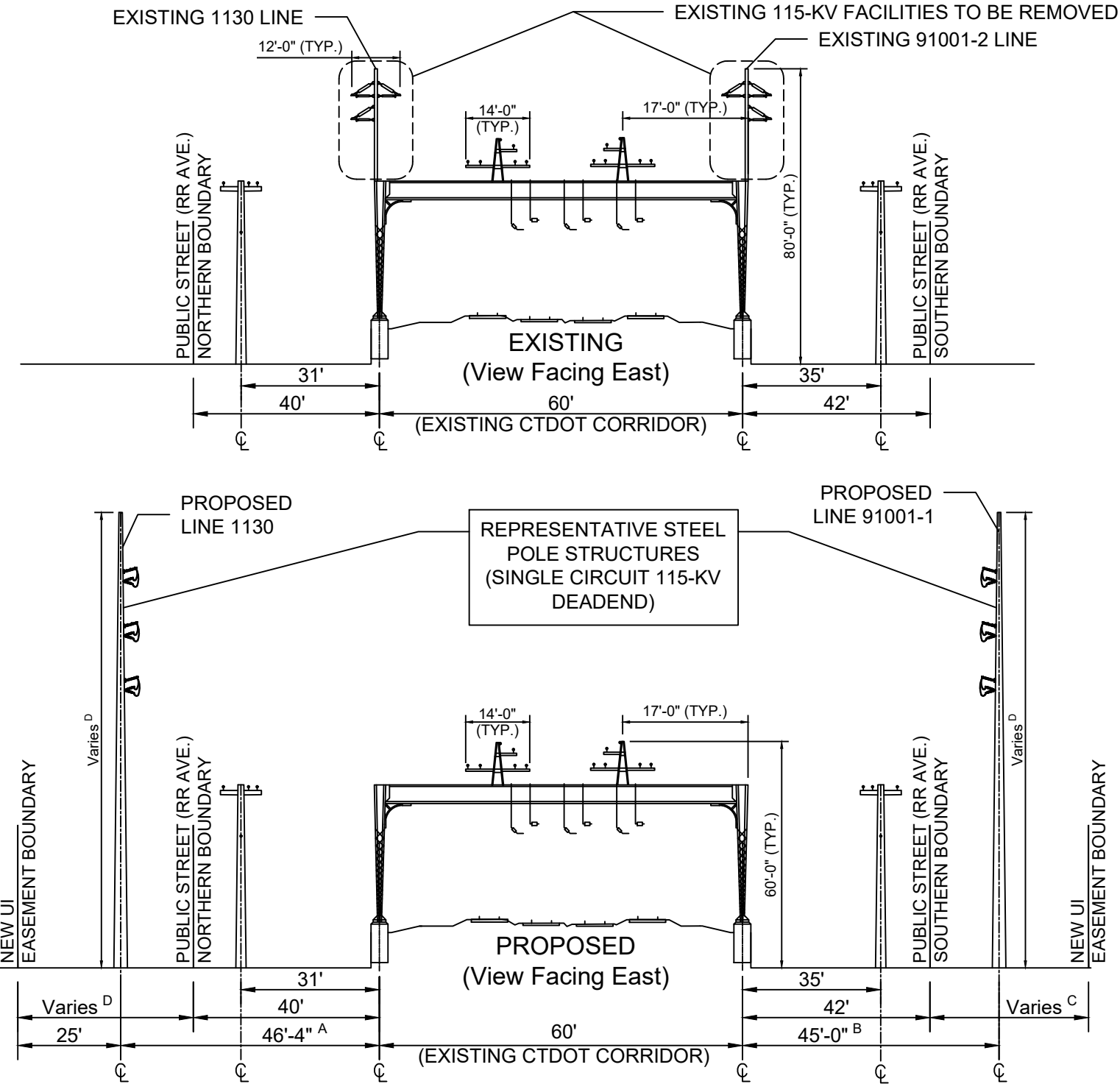
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0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP

DR.	ASW	SCALE: NTS	FILE:	REV.
CK.	MSP	NO.		0-0C
APP.	---	XS-15		
DATE:	4/15/2022			

CROSS SECTION 16  
ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)  
STRS P752N/P752S, STR P760N TO STR P762N & STR P760S TO STR P762S - 0.13 MILES

NOTES:

1. DEPICTED STRUCTURES ARE STEEL DEADEND STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, INSET 17' FROM BOTH THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
3. THE CT DOT CORRIDOR CONTAINS FOUR (4) ELEVATED RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD, AND IS BOUND ON BOTH THE NORTH AND SOUTH SIDES BY A PUBLIC STREET (RAILROAD AVENUE).
4. TO MAINTAIN A.D.A. COMPLIANCE IN CITY SIDEWALKS, IT WAS DEEMED NECESSARY TO PLACE THE PROPOSED MONOPOLES ON PRIVATE PROPERTY NORTH OR SOUTH OF THE SIDEWALK AND EXISTING UI DISTRIBUTION LINE.
5. UI DISTRIBUTION LINES EXIST WITHIN PUBLIC SPACE, BOTH NORTH AND SOUTH OF RAILROAD AVENUE. THESE DISTRIBUTION LINES WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.
6. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM EACH STRUCTURE CENTER. IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.



<sup>A</sup> THIS DISTANCE AVERAGES 46'-4" BUT CAN RANGE FROM 44'-0" TO 48'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>B</sup> THIS DISTANCE AVERAGES 45'-0" BUT CAN RANGE FROM 37'-0" TO 50'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHT OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 21 FOR EXACT STRUCTURE HEIGHTS.

PE Stamp

TYPICAL CROSS SECTION DIAGRAMS

SHEET 17 OF 21

UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS

REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:

DRAWING PREPARED BY: ACCEPTED BY OE:

0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW		MSP
0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW		MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW		MSP
REV.	DATE	BY	DESCRIPTION	APP.	

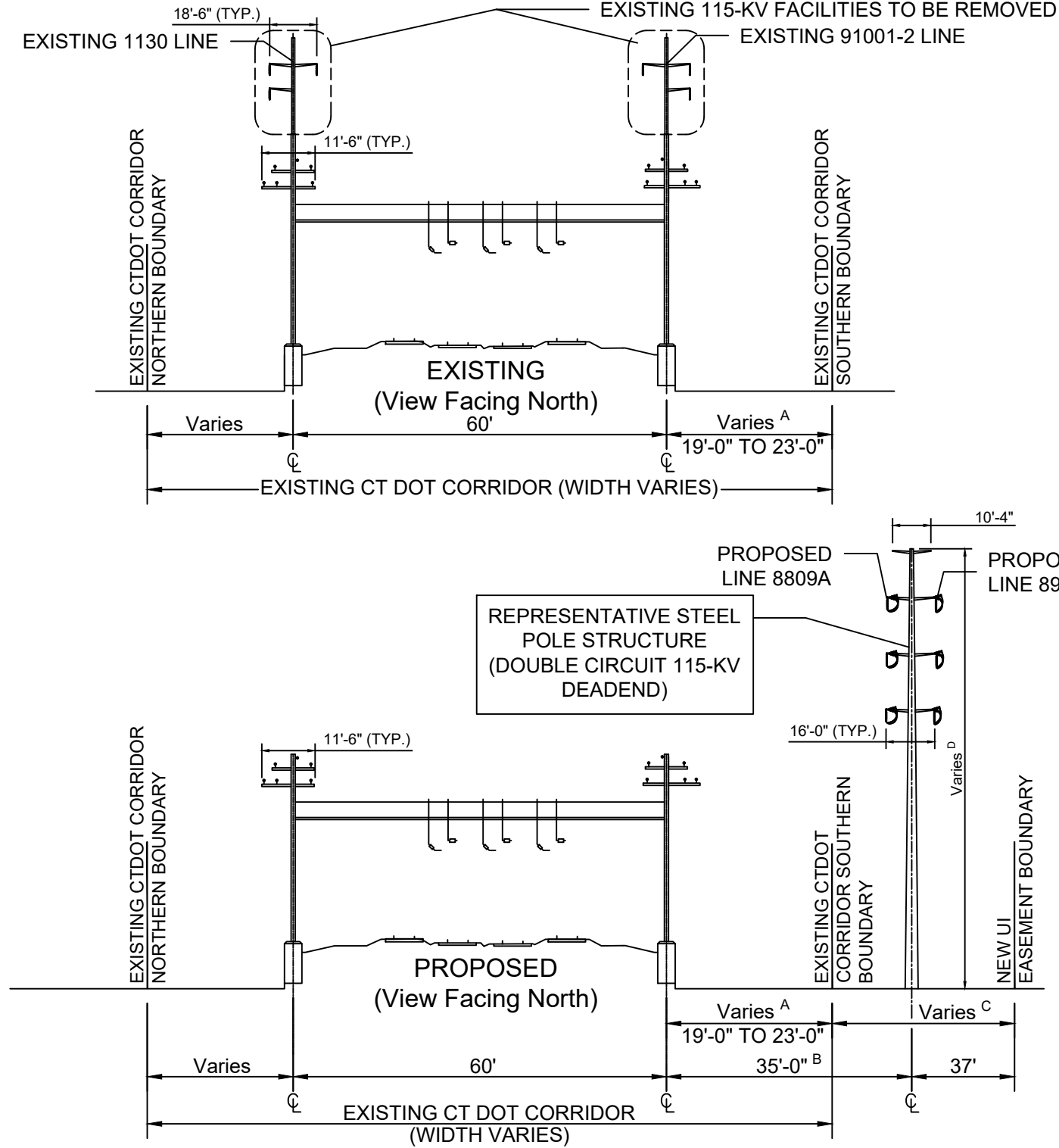
DR.	ASW	SCALE: NTS	FILE:	REV.
CK.	MSP	NO.		0-0C
APP.	---	XS-16		
DATE:	4/15/2022			

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ANSI B



CROSS SECTION 17  
PEQUONNOCK SUBSTATION TO CONGRESS STREET SUBSTATION (BRIDGEPORT)  
STR P779S TO STR P783S - 0.25 MILES



- NOTES:
1. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, BOTH ON THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
  2. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
  3. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
  4. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 37'-0" FROM EACH STRUCTURE CENTER (29'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
  5. DISTANCE FROM NORTHERN CATENARY STRUCTURE TO CT DOT CORRIDOR NORTHERN BOUNDARY WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.

<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 35'-0" BUT CAN RANGE FROM 30'-0" TO 40'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT STRUCTURE HEIGHTS.

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ANSI B

										PE Stamp							TYPICAL CROSS SECTION DIAGRAMS				
REV.	DESCRIPTION			DATE	BY	CK	APP	OWNER ENGINEER:		APPROVAL STAMP:		SHEET 18 OF 21									
												UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS									
								DRAWING PREPARED BY:		ACCEPTED BY OE:		DR. ASW      SCALE: NTS      FILE:      REV. 0-0C									
												XS-17									
												DATE: 4/15/2022									
REV.	DATE	BY	DESCRIPTION				APP.	DATE: 4/15/2022													
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0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW				MSP														

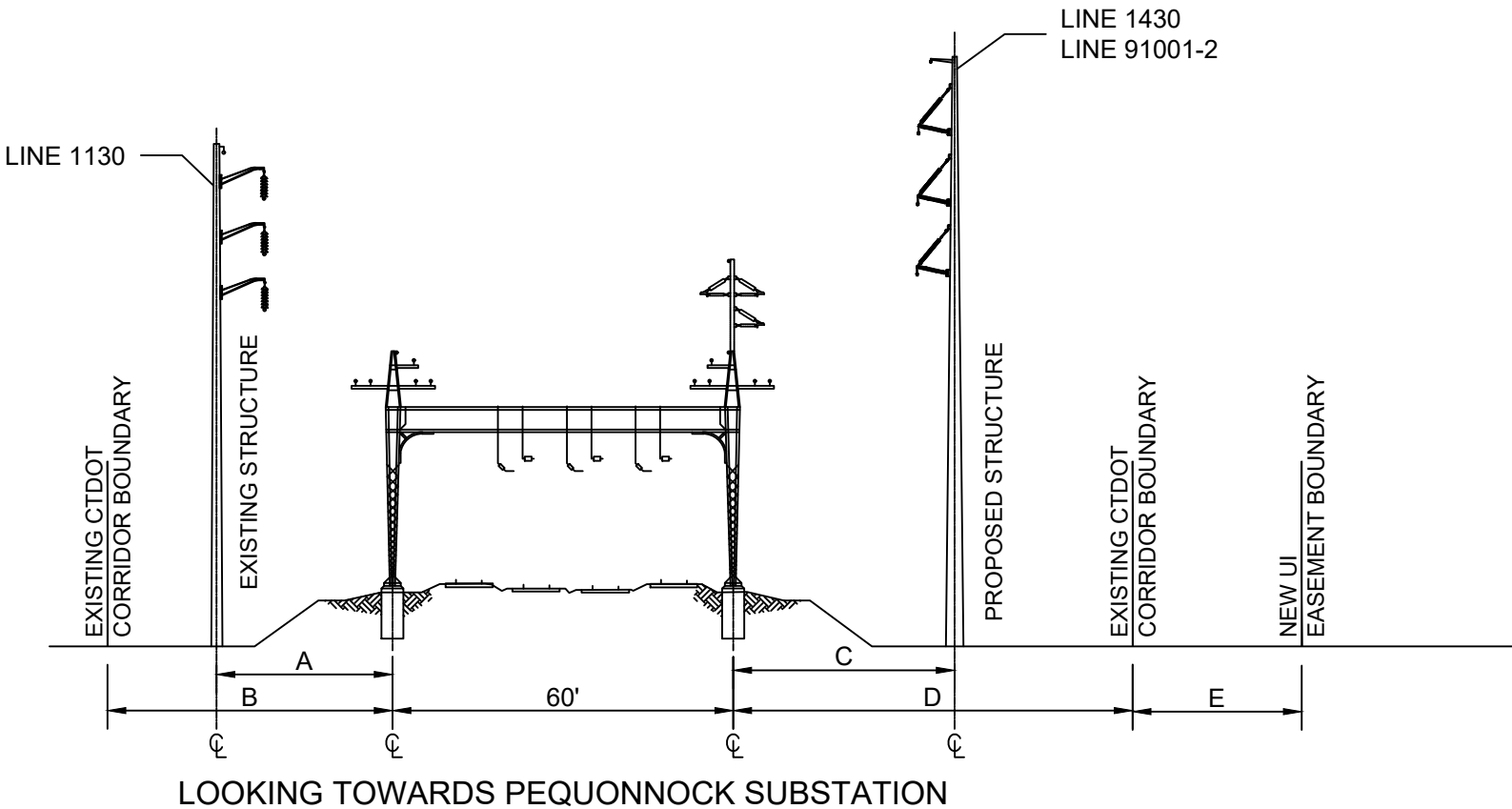
## LIST OF PROPOSED STRUCTURES BY CROSS SECTION REFERENCE

Structure	Cross Section #	Structure Height	A	B	C	D	E
P648S	1	100	4	7	19	39	-
P649S	1	105	4	7	18	39	-
P650S	1	105	3.5	6.5	19	39	-
P651S	1	110	4	7	21.5	39.5	-
P659S	1	135	0	7	9	48	-
P684S	1	120	3	100+	14	70	-
P685S	1	120	4	100+	10	76	-
P686S	1	125	3	100+	4	77	-
P655S	2	115	3	27	26	16.5	27.5
P656S	2	105	2.5	26.5	20	15.5	22.5
P664S	2	130	9	43	22	21	19
P699S	2	120	3	100+	19	18.5	18.5
P700S	2	110	3	100+	51	19	50
P701S	2	110	3	100+	25	18.5	24.5
P703S	2	105	3	100+	26	18.5	25.5
P708S	2	125	3	71	25	19	24
P657S	3	105	0	25	32	24.5	25.5
P681S	3	125	4	7	23	22	19
P661S	4	135	0	6	21	24.5	14.5
P668S	4	110	3	10	10	15	13
P669S	4	110	3	11	19	37	-
P671S	4	120	2	12	25	37.5	5.5
P679S	4	130	4	8	12	18.5	11.5
P682S	4	130	4	4	19	22	15
P663S	5	125	3	33.5	21	42.5	-
P666AS	5	110	0	100	25	100+	-
P688S	5	125	7	100+	13	100+	-
P706S	5	125	0	100+	29	69	-

Structure	Cross Section #	Structure Height	A	B	C	D	E
P665BS	6	125	6	42.5	23	35	6
P673S	6	125	3.5	8.5	18	24	12
P675S	6	115	3	8	17	30	5
P676S	6	105	3	8	17	30	5
P677S	6	105	4	8	16	18	16
P678S	6	120	4	8	10	18	10
P689S	6	135	5	16	16	22.5	11.5
P690S	6	120	4	16	18	21.5	14.5
P691S	6	110	4.5	19	18	22	14
P692S	6	120	4	20	17	21.5	13.5
P693S	6	125	4.5	20	18	21.5	14.5
P695S	6	125	4.5	19	20	20	18
P696S	6	125	4	19	15	19.5	13.5
P698S	6	130	3	100+	18	20	16
P704S	6	120	6	100+	63	68	13
P709S	6	110	3	71	12	20	10
P710AS	6	100	3	67	38	43	13
P710S	6	110	3	21	11	19	10
P711AS	6	100	0	67	15	18.5	14.5
P712S	6	105	3	67	12	18.5	11.5
P713S	6	110	0	67	10	19	9

Structure	Cross Section #	Structure Height	A	B	C	D	E
P716S	9	120	3	20	36	19	35
P721ES	9	130	2	19	16.5	19	15.5
P723S	9	125	2	19	17	6	29
P724S	9	105	2	28	17	18.5	16.5
P719S	10	115	3	19	10	100+	-
P725S	11	110	0	28	11	19	10
P726S	11	100	0	25	12	20	10
P727S	11	110	2	28	10	19	9
P728S	11	120	6	28	19	20	17
P730S	12	130	3	27	9	21	6
P733S	12	130	0	34	15	21	12

A - DISTANCE FROM CATENARY STRUCTURE TO EXISTING POLE (NORTH SIDE)  
B - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (NORTH SIDE)  
C - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (SOUTH SIDE)  
D - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (SOUTH SIDE)  
E - WIDTH OF PROPOSED UT EASEMENT (WHERE APPLICABLE)



REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:
						DRAWING PREPARED BY:	ACCEPTED BY OE:

PE Stamp						TYPICAL CROSS SECTION DIAGRAMS					
						SHEET 19 OF 21					
						UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS					
	0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	DR.	ASW	SCALE: NTS	FILE:		
	0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	CK.	MSP	NO.			REV.
	0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	APP.	---	XS DIMENSIONS TABLE 1			0-0C
	REV.	DATE	BY	DESCRIPTION	APP.	DATE:	4/15/2022				

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ANSI B

## LOOKING TOWARDS PEQUONNOCK SUBSTATION

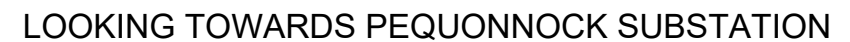


Diagram illustrating the proposed structure and easement boundary. The diagram shows a plan view of a structure with a 60' width. Key boundaries and dimensions are labeled:

- EXISTING CTDOT CORRIDOR BOUNDARY** (Left and Right)
- PROPOSED STRUCTURE** (Central area)
- NEW UI EASEMENT BOUNDARY** (Right side)
- LINE 1130** (Top right boundary)
- LINE 91001-1** (Top right boundary)
- Dimensions:**
  - A:** Distance from left boundary to structure start.
  - B:** Distance from structure end to right boundary.
  - C:** Total width of the structure (60').
  - D:** Distance from structure end to easement boundary.

## TYPICAL CROSS SECTION DIAGRAMS

UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS

0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
REV.	DATE	BY	DESCRIPTION	APP.

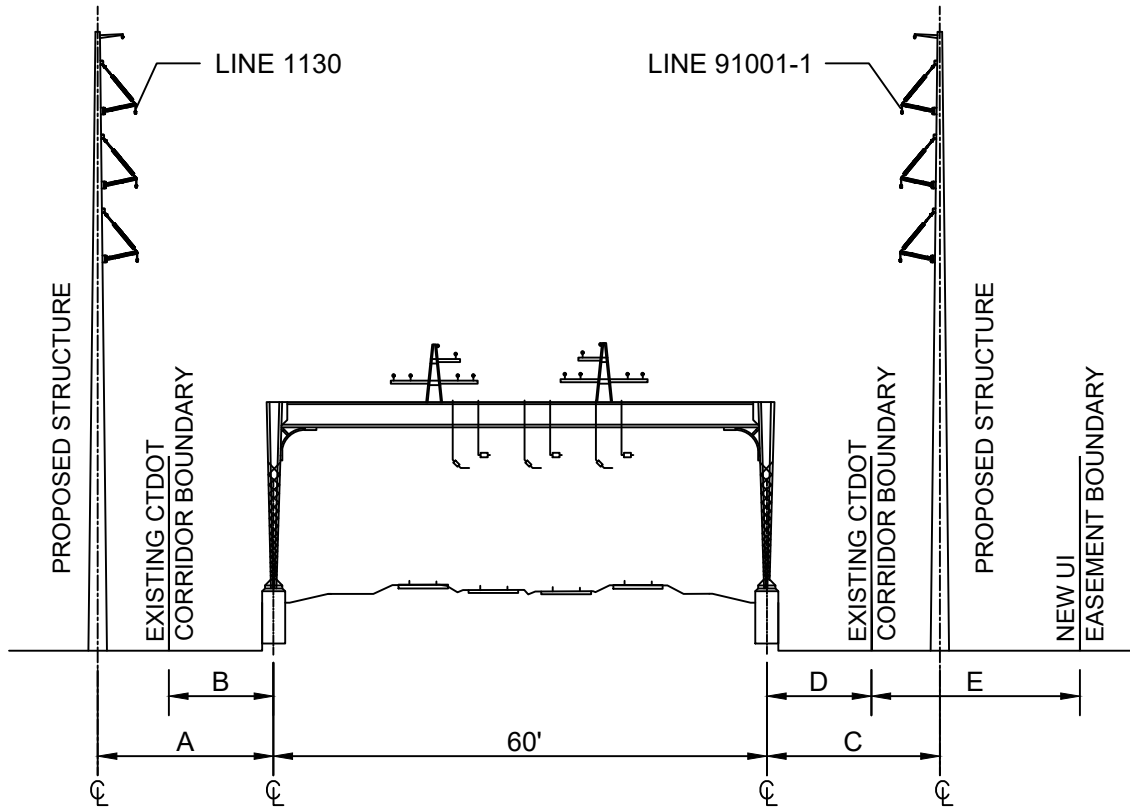
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CK.	MSP	NO.		REV.
APP.	---	XS DIMENSIONS TABLE 2		0-0C
DATE:	4/15/2022			

LIST OF PROPOSED STRUCTURES BY CROSS SECTION REFERENCE

Structure	Cross Section #	Structure Height	A	B	C	D	E
P756N	15	130	57	0	-	0	-
P756S	15	140	-	0	45	0	63
P757N	15	130	135	0	-	0	-
P758N	15	125	206	0	-	0	-
P758S	15	145	-	0	48	0	66
P759N	15	125	317	0	-	0	-
P760S	15	140	-	0	45	0	63
P752N	16	125	43	0	-	0	-
P752S	16	130	-	0	37	0	55
P760N	16	130	48	0	-	0	-
P762N	16	125	47	0	-	0	-
P760S	16	140	-	0	48	0	66
P762S	16	125	-	0	50	0	68

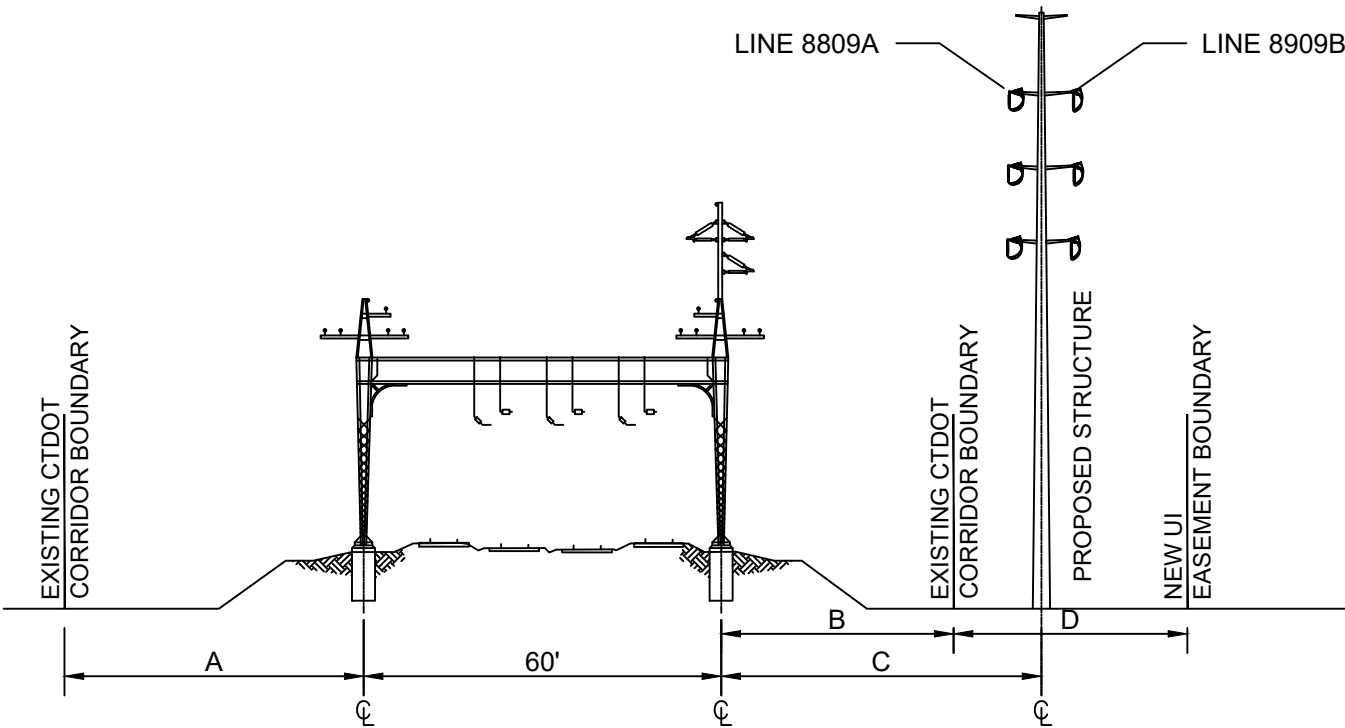
Structure	Cross Section #	Structure Height	A	B	C	D
P779S	17	190	23	19	40	58
P783S	17	130	22	23	30	44

A - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (NORTH SIDE)  
C - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (SOUTH SIDE)  
B,D - THE RAILROAD CORRIDOR BOUNDARY IS AT THE EDGE OF THE CATENARY STRUCTURE  
E - WIDTH OF PROPOSED EASEMENT



LOOKING TOWARDS PEQUONNOCK SUBSTATION

A - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (NORTH SIDE)  
B - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (SOUTH SIDE)  
C - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (SOUTH SIDE)  
D - WIDTH OF PROPOSED EASEMENT



LOOKING TOWARDS PEQUONNOCK SUBSTATION

PE Stamp						TYPICAL CROSS SECTION DIAGRAMS			
						SHEET 21 OF 21			
						UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
		DR. ASW		SCALE: NTS		FILE:			
		CK. MSP		NO.				REV.	
		APP. ---				XS DIMENSION TABLE 3		0-0C	
		DATE: 4/15/2022							

REV.	DATE	BY	DESCRIPTION	APP.
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0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP

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